

§ 7.2 OFF-STREET PARKING, LOADING, AND STACKING

7.2.1 Purpose

The purpose of this Section is to:

- A. Ensure that each development within the City provide adequate and reasonable parking, stacking, and loading spaces necessary to serve the development.
- B. Allow for flexibility in the design of parking areas.
- C. Protect adjacent areas and the general public from the:
 - 1. Noise, glare of headlights, dust and fumes resulting from the operation of motor vehicles.
 - 2. Glare and heat from parking areas.
 - 3. Lack of visual relief from expanses of paving and accelerated run-off of surface water from land covered by impervious surfaces.

7.2.2 Minimum Off-Street Parking Requirements

A. Required Vehicle Spaces

- 1. Table 7.2.2: Off-Street Vehicle Parking Requirements states the minimum number of off-street parking spaces to be provided for the designated uses. Table 7.2.2 lists parking requirements for the uses listed within the districts. These uses are listed only for the purposes of this Section and do not indicate whether such uses are allowed within any district. Certain uses listed within the districts may not have parking requirements.
- 2. Where the applicant feels the parking ratios are in excess of what is required, data submitted by the applicant may be used to determine the appropriate ratio for the specific proposed use. Such data may include site studies from similar uses, generally-accepted engineering standards (for example, Institute of Transportation Engineers trip rates or parking generation), or independent engineering calculations based on the nature of the proposed use. The Director of Development Services must evaluate such submittals to determine an acceptable ratio for the proposed use.
- 3. The amount of required vehicle spaces must be met when the following actions are undertaken:
 - a. New construction of a principal building, including construction of additional principal buildings within an existing development.
 - b. A change in use.
 - c. Any increase in ground level area by 1,000 square feet or more of existing structures up to 10,000 square feet, or any increase in the floor area by 10% or more of existing structures greater than 10,000 square feet.
 - d. Destruction of more than 50% of an existing nonconforming structure, as defined by Section 9.4, whose reconstruction does not qualify for an exception under Section 9.2.1.E, must comply with all parking requirements and must be treated as new development. If destruction is less than 50%, compliance is not required unless the ground level floor area is increased as described in item c above.
- 4. The parking space requirements for a use not addressed in the chart table must be established by the Director of Development Services according to professionally acceptable standards and practices.

B. Calculation of Required Vehicle Spaces

- 1. The total number of required vehicle spaces is calculated by the principal use of the lot. When more than one use occupies the same lot, the number of required spaces is the sum of the separate requirements for each use.

2. Unless otherwise expressly stated, all square footage based parking and loading standards must be computed on the basis of gross floor area (GFA). The gross floor area of a building must be measured from the exterior faces of the walls and from the centerline of the walls separating two buildings but excludes stairwells, elevator shafts, mechanical rooms, and any facilities reserved for employee-only use, such as lounges and kitchens/lunchrooms.

3. Where fractional spaces result, the parking spaces required must be rounded up to the next whole number.

C. Districts Exempt from Minimum Parking Requirements

The following districts are exempt from minimum parking requirements:

1. CBD Central Business District
2. CMU Corridor Mixed-Use District

| Table 7.2.2: Off-Street Vehicle Parking Requirements | |
|--|---|
| Principal Uses | Minimum Parking Requirements |
| Residential | |
| Dwelling - Single-Family Detached | 2 per unit |
| Dwelling - Two-Family | 2 per unit |
| Dwelling - Townhouse | 2 per unit |
| Dwelling - Multi-Family | Studio or 1 bedroom: 1 space More than 1 bedroom: 2 per unit Plus 1 per 6 units for visitor parking |
| Dwelling - Above the Ground Floor | Studio or 1 bedroom: 1 space More than 1 bedroom: 2 per unit Plus 1 per 6 units for visitor parking |
| Community Home for Persons with Disabilities | 1 per 2 bedrooms |
| Cluster Development | <i>Based on dwelling type</i> |
| Cottage Court Development | <i>Based on dwelling type</i> |
| Family Child Care Home | 2 per unit |
| Foster Family Home | 2 per unit |
| Foster Group Home | 1 per 4 bedrooms |
| Group Home I | 1 per 4 bedrooms |
| Group Home II | 1 per 4 bedrooms |
| Manufactured Home Park | 2 per manufactured home site |
| Manufactured Home Subdivision | 2 per manufactured home or dwelling unit |
| Commercial and Service | |
| Adult Gaming Establishment | 1 per 100sf GFA |
| Amusement Facility - Indoor | 1 per 100sf GFA |
| Amusement Facility - Outdoor | All uses with fixed seats: 1 per 3 seats or seating spaces All uses without fixed seats: 1 per 250sf GFA |
| Animal Care Facility | 1 per 500sf GFA |
| Animal Shelter | 1 per 1,000sf GFA of indoor area |
| Art Gallery | 1 per 500sf GFA |
| Arts Studio | 1 per 500sf GFA |
| Bar | 1 per 100sf GFA |
| Boat or Recreational Vehicle Sales | 1 per 500sf of indoors GFA + 1 per 10,000sf of outdoor lot area |
| Boat or Recreational Vehicle Storage | 1 per 500sf GFA of office space |
| Broadcast Facility - With Antenna | 1 per vehicle normally required to service the utility |
| Broadcast Facility - Without Antenna | 1 per 500sf GFA |
| Car Wash | 1 per bay |
| Car Wash, Self-Service | 0.5 per bay |
| Child Care Center | 2 per unit |
| Commercial Kitchen | 1 per 500sf GFA |
| Convention Center | 1 per 1,000sf GFA |
| Day Activity and Health Services (DAHS) Facility | 1 per 500sf GFA |
| Financial Institution, Traditional | 1 per 500sf GFA |
| Financial Institution, Alternative | 1 per 500sf GFA |
| Funeral Home | 1 per 250sf GFA of assembly areas, including office space |
| Game Processing | 1 per 1,000sf GFA |

| Table 7.2.2: Off-Street Vehicle Parking Requirements | |
|---|--|
| Principal Uses | Minimum Parking Requirements |
| Gas Station | 1 per 250sf GFA of retail area + 1 per pump island |
| Heavy Retail, Rental and Service Establishment | 1/500sf of indoors GFA + 1 per 5,000sf of outdoor lot area |
| Hotel/Motel | 1 per room |
| Industrial Design | 1 per 500sf GFA |
| Gym or Fitness Studio | 1 per 500sf GFA |
| Kennel | 1 per 1,000sf GFA of indoor area |
| Live Performance Venue | 1 per 100sf GFA |
| Lodge/Meeting Hall | 1 per 500sf GFA |
| Medical/Dental Clinic | 1 per 500sf GFA |
| Micro-Production of Alcohol | 1 per 100sf GFA of public area + 1 per 1,000sf GFA of brewing facilities |
| Nightclub | 1 per 100sf GFA |
| Office | 1 per 500sf GFA |
| Personal Service Establishment | 1 per 500sf GFA |
| Reception Facility | 1 per 100sf GFA |
| Research and Development (R&D) | 1 per 500sf GFA |
| Restaurant | 1 per 100sf GFA |
| Retail Goods Establishment | 1 per 500sf GFA |
| Self-Storage Facility: Climate-Controlled | 1 per 25 storage units |
| Self-Storage Facility: Outdoor | 1 per 25 storage units |
| Sexually-Oriented Business | 1 per 100sf GFA |
| Smoking Lounge | 1 per 100sf GFA |
| Specialty Food Service | 1 per 100sf GFA of public area + 1 per 1,000sf GFA of production facilities |
| Truck Stop | 1 per 500sf GFA of indoor facilities |
| Vehicle Dealership: Enclosed | 1 per 500sf GFA of indoor area |
| Vehicle Dealership: Outdoor | 1 per 500sf GFA of indoor area + 1 per 10,000sf outdoor lot area |
| Vehicle Rental: Enclosed | 1 per 500sf GFA of indoor area |
| Vehicle Rental: Outdoor | 1 per 500sf GFA of indoor area |
| Vehicle Repair/Services: Limited | 4 per bay |
| Vehicle Repair/Services: Major | 4 per bay |
| Industrial | |
| Airport | 1 per 100sf GFA of terminal area |
| Borrow Pit | 1 per 500sf GFA of office area |
| Freight Terminal | 1 per 500sf GFA of office area |
| Hazardous and Flammable Materials Storage (Principal Use) | 1 per 500sf GFA of office area |
| Industrial, Craft | 1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf |
| Industrial, General | 1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf |
| Industrial, Light | 1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf |
| Light Assembly | 1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf |
| Movie Studio | 1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf |
| Outdoor Storage Yard | 1 per 500sf GFA of office space |
| Quarry | 1 per 500sf GFA of office area |
| Recycling Station | 1 per 500sf GFA of office area |
| Salvage/Junk Yard | 1 per 500sf GFA of office area |
| Warehouse, Distribution | 1/500sf GFA of office area + 1/15,000sf GFA of warehouse |
| Waste Management Facility | 1 per 500sf GFA of office area |
| Wholesale Goods Establishment | 1/1,000sf GFA |
| Institutional and Social Service | |
| Community Center | 1 per 500sf GFA |
| Cultural Facility | 1 per 500sf GFA |
| Detention Facility | 1 per 1,000sf GFA |

| Table 7.2.2: Off-Street Vehicle Parking Requirements | |
|--|--|
| Principal Uses | Minimum Parking Requirements |
| Drug Treatment Clinic | 1 per 500sf GFA |
| Educational Facility - Pre-School | 1 per 500sf GFA |
| Educational Facility - Primary/ Secondary | 1 per 500sf GFA of office area + 2 per classroom |
| Educational Facility - University/ College | 1 per 500sf GFA of office area + 10 per classroom |
| Educational Facility - Vocational | 1 per 500sf GFA |
| Food Bank | 1 per 500sf GFA of office area |
| Food Pantry | 1 per 500sf GFA |
| Government Office/Facility | 1 per 500sf GFA |
| Homeless Shelter | 1 per 500sf GFA of office area |
| Hospital | 1 per 3 beds |
| Place of Worship | 1 per 500sf GFA |
| Public Safety Facility | 1 per 500sf GFA |
| Public Works Facility | 1 per 500sf GFA |
| Residential Care Facility | 2 per patient room |
| Residential Addiction Treatment Facility | 2 per patient room |
| Social Service Facility | 1 per 500sf GFA |
| Open Space and Agricultural | |
| Campground | 1 per campsite |
| Cemetery | 1 per 500sf of office area and chapel |
| Commercial Stable | 1 per 2 horse stalls |
| Golf Course/Driving Range | 3 per hole or tee + 1/500sf GFA of indoor public areas |
| Marina | 1 space per 2 wet boat slips |
| Private Recreation Club | 1 per 500sf GFA of indoor area |
| Recreational Vehicle (RV) Park | 1 per 4 pads |
| Utility, Infrastructure, and Transportation | |
| Passenger Terminal | 1 per 500sf GFA of terminal area |
| Vehicle Operations Facility | 5 per bay or 1 per 250sf GFA, as applicable and whichever is greater |

7.2.3. Parking Flexibilities

A. Permitted Reductions of Minimum Parking Requirements

1. RC District

Required parking spaces may be reduced by 50% within the RC Resort Commercial District for all uses except for hotel/motel.

2. Permeable Pavement

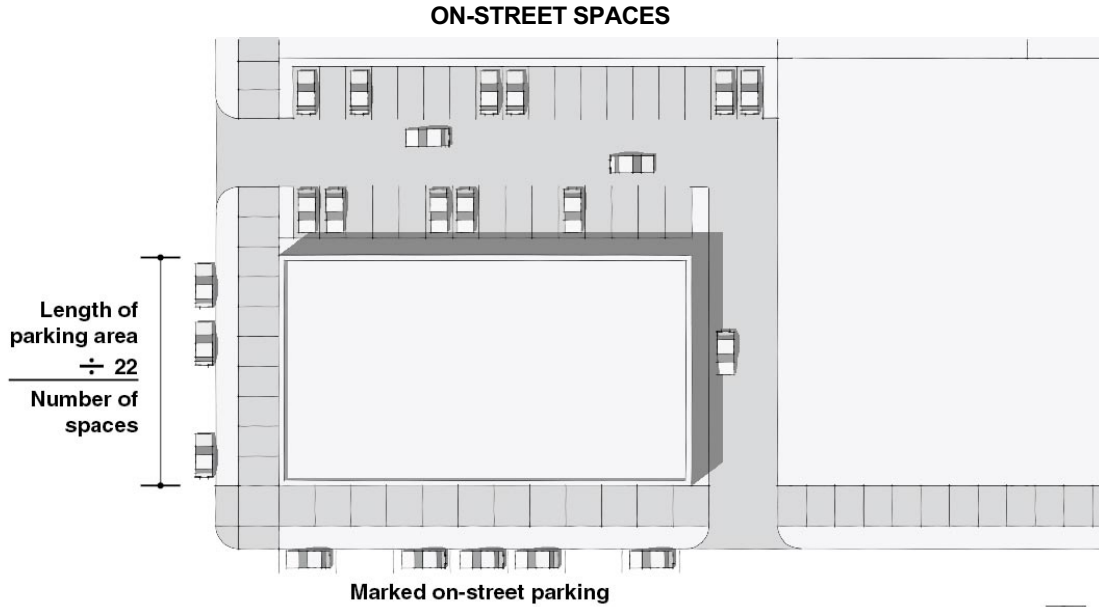
Where 80% of the total parking area is constructed of permeable pavement, required parking spaces may be reduced by 20%. Permeable pavement includes any materials installed, operated, and maintained to permit the passage of water through the pavement, including, but not limited to, porous concrete, porous asphalt, permeable interlocking concrete pavers, and concrete grid pavers.

B. Use of On-Street Spaces

In the commercial and mixed-use districts (Section 4.5), on-street parking spaces located along the front or side lot line may be counted toward required off-street parking spaces for commercial uses. New on-street parking spaces may also be created to count toward required off-street parking but must be located along the street lot line, and must be accessible 24 hours a day to the public.

1. Where on-street parking spaces are unmarked, the number of parking spaces is calculated by dividing the length of the on-street parking area located parallel to the lot line of the property under consideration divided by 22, where a fraction of less than one-half is disregarded, and a fraction of one-half or more is counted as one space.

2. Where on-street parking spaces are marked, each marked space counts as one required parking space, including any space where at least 75% of the width is located along the lot line of the property under consideration.



C. Multi-Tenant Developments

For multi-tenant developments (two or more uses on one site), off-street parking spaces for uses within the development may be provided collectively at a reduced amount of the total number of spaces, provided it meets the calculation of Table 7.2.3: Multi-Tenant Parking Calculation.

1. The required number of spaces for each use is calculated according to Table 7.2.3.
2. The required number of spaces for each use is then applied to the percentages for each timeframe, according to the appropriate land use category, in Table 7.2.3 to determine the number of required spaces. This is done for each timeframe category.
3. The numbers are summed for all uses within each timeframe and the highest sum total in a timeframe is the required number of spaces.
4. Where fractional spaces result, the parking spaces required be rounded up to the next whole number.

Table 12-2: Multi-Tenant Parking Calculation

| Land Use Category | Weekday | | | Weekend | | |
|--|---------|---------|---------|---------|---------|---------|
| | Mid-7am | 7am-6pm | 6pm-Mid | Mid-7am | 7am-6pm | 6pm-Mid |
| Residential | 100% | 100% | 100% | 100% | 100% | 75% |
| Commercial (Unless specifically cited in this Table) | 0% | 100% | 80% | 0% | 100% | 60% |
| Restaurant and/or Bar | 50% | 70% | 100% | 45% | 70% | 100% |
| Hotel/Motel | 100% | 50% | 90% | 100% | 65% | 80% |
| Office | 5% | 100% | 5% | 0% | 40% | 10% |
| Industrial | 5% | 100% | 5% | 0% | 40% | 10% |
| Institutional | 0% | 70% | 0% | 0% | 100% | 0% |

Example: For a multi-use development with the following types of uses within the development, based on current parking requirements, the number of total required spaces is:

| Use & Square Footage | Parking Requirement | Number of Spaces Needed |
|--------------------------|---------------------|-------------------------|
| Commercial: 15,000sf GFA | 1 per 500sf GFA | 30 spaces |
| Restaurant: 20,000sf GFA | 1 per 100sf GFA | 20 spaces |
| Hotel/Motel: 90 rooms | 1 per room | 90 spaces |
| Office: 40,000sf GFA | 1 per 500sf GFA | 80 spaces |

| | |
|------------------------------|-------------------|
| TOTAL SPACES REQUIRED | 220 spaces |
|------------------------------|-------------------|

Using the shared parking calculation, these numbers for each of the uses are plugged into the table and using the percentages allotted to each land use for each time of day. For example, a commercial use requires 30 spaces; on a weekday, during the Midnight to 7am timeframe the percentage is 0%, therefore no spaces are required in that time slot.

Once all timeframes are calculated for each use, the number of spaces are totaled per timeframe.

| Land Use Category | Weekday | | | Weekend | | |
|-----------------------|------------|------------|------------|-----------|------------|------------|
| | Mid-7am | 7am-6pm | 6pm-Mid | Mid-7am | 7am-6pm | 6pm-Mid |
| Residential | - | - | - | - | - | - |
| Commercial | 0 | 30 | 24 | 0 | 30 | 18 |
| Restaurant and/or Bar | 10 | 14 | 20 | 9 | 14 | 20 |
| Hotel/Motel | 90 | 45 | 81 | 90 | 59 | 72 |
| Office | 4 | 80 | 4 | 0 | 32 | 8 |
| Industrial | - | - | - | - | - | - |
| Institutional | - | - | - | - | - | - |
| Totals | 104 | 169 | 129 | 99 | 135 | 118 |

With a straight parking calculation, 220 spaces are required. However, the multi-tenant parking provision allows this example development to be constructed by-right with 169 spaces (the highest total number of spaces within the various timeframes – specifically, the 7am to 6pm weekday timeframe). This is because these timeframe calculations take into account the times of day the various uses utilize the most parking.

D. Cross-Access

1. Adjacent nonresidential uses that possess dedicated parking areas are encouraged to provide a cross-access drive to allow circulation between sites.
2. If cross-access is provided, a parking reduction of 10% is permitted for each use accessed by the cross-access driveway.
3. The property owners must provide a written agreement between all users of the cross-access driveway to the Director of Development Services. The agreement must include a joint maintenance agreement defining the maintenance responsibilities of each property owner. Such agreement must be recorded with the County.
4. Cross-access driveways must meet the following standards:
 - a. A minimum width of 24 feet is required with no adjacent parking to ensure two-way travel aisles to accommodate automobiles, service vehicles, and loading vehicles.
 - b. Bump-outs and other design features are required to make it visually obvious that the abutting properties are tied together.
 - c. A unified access and circulation plan is required for coordinated or shared parking areas.

E. Reduction for Bicycle Facilities

The Director of Development Services may authorize a reduction up to a maximum of 20% of required off-street parking spaces for development or uses that make special provisions to accommodate bicyclist facilities. Examples of accommodations include secure bicycle lockers (i.e., storage that includes a locking mechanism that is accessed by electronic card or other similar security feature), employee shower facilities, and dressing areas for employees. If secure bicycle lockers are included, they may also be counted toward required bicycle spaces per Section 7.2.5 below.

F. Golf Cart Spaces in the IO Overlay District

In the IO Island Overlay District only, up to a maximum of 10% of required off-street parking spaces may be designed to accommodate golf carts.

7.2.4 Permissions for Off-Site Parking and Shared Parking

Off-site spaces may be located on a separate lot from the lot on which the principal use is located if approved by the Director of Development Services and if the off-site parking area complies with the following standards. In addition, shared parking is permitted where one of the uses is off-site.

A. Permissions

Off-site parking is permitted in the commercial and mixed-use districts with the following exceptions:

1. Required parking spaces reserved for persons with disabilities cannot be located off-site.
2. Residential uses cannot locate required parking off-site in the following districts, including residential dwellings within a mixed-use development: NC, NO, RP, and RC Districts.

B. Location

1. Off-site parking spaces must be located within 2,000 feet from the property served for nonresidential uses or 750 feet from the property served for residential uses, measured from the primary entrance of the use served.
2. A greater distance may be permitted for nonresidential uses if a dedicated shuttle bus service is provided to the remote parking area.
3. In order to qualify, the shortest dedicated walkways cannot require a crossing of an arterial street.
4. Off-site parking areas serving uses located in nonresidential zoning districts must be located in nonresidential zoning districts.
5. Off-site parking areas serving uses located in residential zoning districts may be located in residential or nonresidential zoning districts.

C. Agreement for Off-Site Parking

1. In the event that an off-site parking area is not under the same ownership as the principal use served, a written agreement between the record owners is required.
2. The owner of the off-site parking area enter into a written agreement with the City, with enforcement running to the City, providing that the land comprising the parking area will never be conveyed except in conjunction with the sale of the building which the parking area serves or subject to such facility requirements; that the owner agrees to bear the expense of recording the agreement; and that such agreement binds their heirs, successors, and assigns. Such agreement must be recorded with the County.
3. An off-site parking agreement may be rescinded only if all required off-street parking spaces will be provided in accordance with this Code.

D. Shared Parking

1. An off-street parking facility may be shared between two uses, where one use is off-site, provided that use of such facility by each user does not occur at the same time. The number of parking spaces within the facility must meet the requirement for the use with the greater number of required spaces.
2. Approval is obtained from the Director of Development Services that confirms that the use of such facility by each user does not take place at the same hours during the same days of the week.
3. The users of the shared parking facility must record an agreement to share parking facilities, subject to approval by the City. A copy of the recorded agreement must be given to the Director of Development Services.
4. All standards of this Section must be met for the user located off-site.

7.2.5 Single and Two-Family Residential Parking Design

- A. The parking of vehicles is prohibited in any front or corner side yard except on a paved surface.
- B. The cumulative area of any impermeable surface area located in any front or corner side yard cannot exceed 50% of the total area of the front or corner side yard.
- C. Residential driveways must be made of an all-weather dust-proof surface, such as asphalt, concrete, or brick.
- D. Lots of five acres or greater with a minimum driveway apron depth of 50 feet from the edge of a street made of an all-weather dust-proof surface are exempt from residential driveway requirements.

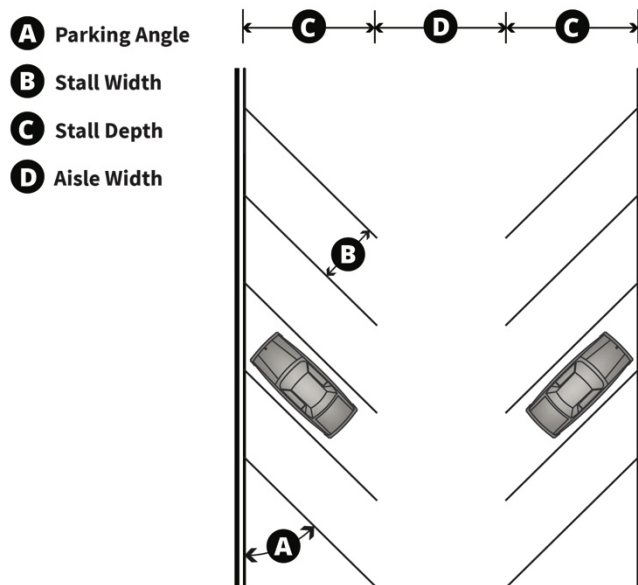
7.2.6 Off-Street Parking Facility Design Standards

A. Dimensions

Off-street parking spaces and drive aisles within a parking facility must be designed in accordance with the minimum dimensions in Table 7.2.5: Off-Street Parking Dimensional Standards. Other parking angles other than those described in Table 7.2.5 are permitted but must be approved by the Director of Development Services and provide evidence of safe and efficient parking configuration and traffic circulation.

| Parking Angle (A) | Minimum Parking Stall Width (B) | Minimum Parking Stall Depth (C) | Minimum Parking Aisle Width (D) |
|-------------------|---------------------------------|---------------------------------|---------------------------------|
| 0° | -- | 22' | 12' / 24* |
| 45° | 9' | 12' | 12' |
| 60° | 9' | 16' | 16' |
| 90° | 9' | 20' | 24** |

* Two-way traffic



B. Access

1. All required off-street parking facilities must have vehicular access from a street, driveway, alley, or cross-access connection.

2. All required off-street parking facilities of 20 or more spaces must have an internal pedestrian circulation system that allows for safe passage between parking areas and any public sidewalk in the adjacent right-of-way and the use it serves. This includes, but is not limited to, interconnected sidewalks, striped walkways, and separated walkways.
3. All parking facilities must be designed with vehicle egress and ingress points that least interfere with traffic movement.
4. Parking facilities must be designed to allow the driver to proceed forward into traffic, rather than back out; this does not apply to single-family and two-family dwellings.
5. All curb cuts must comply with the regulations of the City Code. Townhouse developments are prohibited from constructing individual curb cuts for each dwelling unit along a public street.
6. All driveways must conform to all applicable driveway specifications adopted by the City.
7. Dead end parking lots without a turnaround space are prohibited. A turnaround space must have a minimum depth and width of nine feet, and must be designated with signs stating "No Parking" and painted to indicate parking is prohibited.
8. No portion of a vehicle may encroach or overhang any lot line. A permanently installed curb, wall, or other such physical barrier is required.

C. Surfacing

Off-street parking lots of four or more spaces must be paved with all-weather materials such as asphalt, concrete, or brick. Permeable pavement is also permitted. Permeable paving includes any materials installed, operated, and maintained to permit the passage of water through the pavement, including, but not limited to, porous concrete, porous asphalt, permeable interlocking concrete pavers, and concrete grid pavers.

D. Striping

Off-street parking lots of four or more spaces must delineate parking spaces with paint or other permanent materials, which must be maintained in clearly visible condition.

E. Curbing and Wheel Stops

Curbing and wheel stops are required when a parking space abuts required landscape areas, pedestrian walkways, structures, fences, or the edge of the parking lot along a lot line. Such curbing must be constructed of permanent materials, such as concrete or masonry, a minimum height of four inches above ground level, and permanently affixed to the paved parking area.

F. Landscape

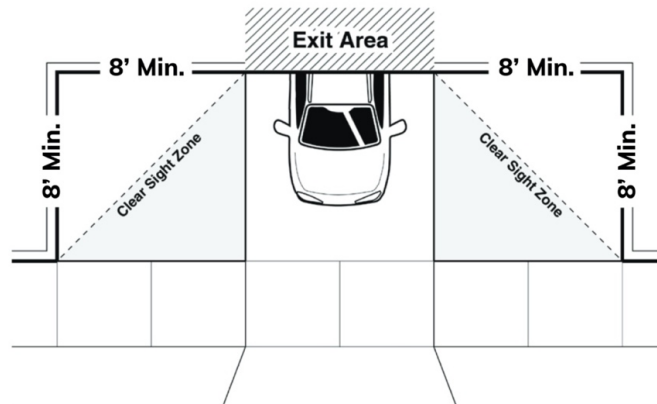
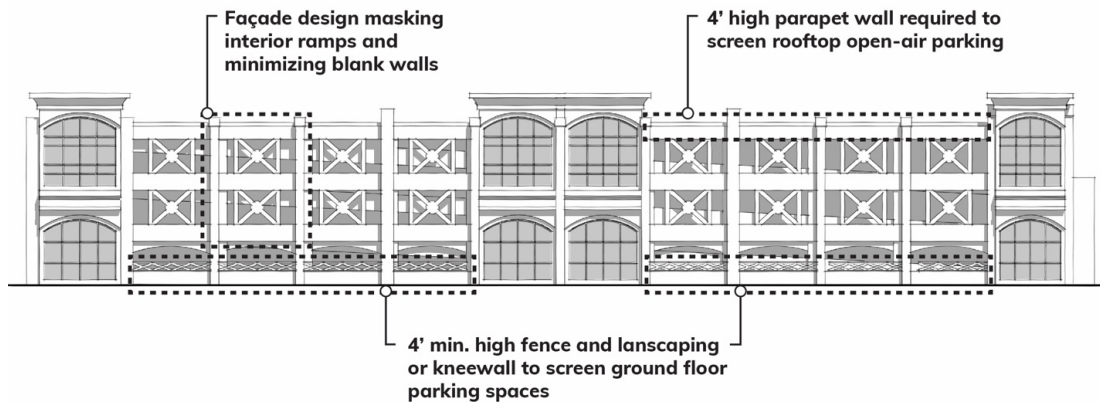
All parking facilities must be landscaped in accordance with Section 7.3.

G. Parking Structure Design

1. On facades that front on public streets, façade design and screening must mask the interior ramps.
2. Parking structures must be designed to minimize blank facades through architectural detail and landscape.
3. On portions of the ground floor façade where parking spaces are visible, a decorative fence and landscape or a kneewall is required to screen parking spaces. Such fence or kneewall must be a minimum of four feet in height.
4. For parking structures with rooftop open-air parking, a four foot parapet wall is required for screening.
5. A vehicular clear sight zone must be included at vehicular exit areas as follows:
 - a. The façade of vehicular exit areas must be set back from any pedestrian walkway along that façade a minimum of eight feet for the portion of the façade that includes the vehicle exit area and eight feet on each side of the exit opening.
 - b. A sight triangle is defined by drawing a line from the edge of the vehicular exit area to a point on the property line abutting the pedestrian walkway eight feet to the side of the exit lane.

- c. In the sight triangle (bound by the parking structure wall, pedestrian walkway and vehicular exit lane), groundcover, landscape, or decorative wall must be used to act as a buffer between the exit aisle and the pedestrian walkway. Landscape or a decorative wall must not exceed three feet in height in order to maintain driver sightlines to the pedestrian walkway.
- d. The upper story façade(s) of the parking structure may overhang the vehicular clear sight zone.

PARKING STRUCTURE DESIGN



H. Design of Controlled Entrances

- 1. A minimum of 40 feet must be provided between any card reader or controlled access point and the lot line.
- 2. Drives must be arranged to provide the ability for a vehicle to turn around without backing into the street

7.2.7 Required Bicycle Parking

A. Bicycle parking, as indicated in Table 7.2.5: Required Bicycle Spaces, is required for the following development actions:

- 1. New construction.
- 2. When a parking lot is fully reconstructed or resurfaced.

3. When a parking lot is expanded by an additional 50% or more spaces (viz., the total number of spaces after expansion is 150% or more of the spaces prior to expansion).

B. Bicycle parking spaces must be provided as indicated in Table 7.2.5.

| TABLE 7.2.5: REQUIRED BICYCLE SPACES | |
|--|--|
| Use | Required Bicycle Spaces |
| Multi-Family Dwelling – less than 8 dwelling units | 0.5 per unit Designed as short-term or long-term spaces |
| Multi-Family Dwelling – 8 or more dwelling units | 0.5 per unit All spaces must be long-term spaces |
| Nonresidential - Over 10,000sf of GFA | 1 per 10 vehicle spaces A minimum of 75% of spaces must be short-term (long-term spaces are not required, i.e., all spaces may be short-term) |

C. When bicycle parking is required, a minimum of four bicycle spaces must be provided. No more than 30 bicycle spaces are required in any instance, though the development has the option to exceed this number if desired.

D. Bicycle parking is not required in the industrial districts.

E. Design of bicycle spaces must meet the following standards:

1. Each required bicycle parking space must be accessible without moving another bicycle. There must be an aisle at least five feet wide between each set of bicycle parking to allow room for bicycle maneuvering.
2. The area devoted to short-term bicycle parking must be surfaced as required for vehicle parking areas.
3. Short-term bicycle parking racks must permit the bicycle frame and one wheel to be locked to the rack and support the bicycle in a stable position. Structures that require a user-supplied locking device must be designed to accommodate U-shaped locking devices. All racks must be securely anchored to the ground or a structure to prevent the racks from being removed from the location.
4. All long-term bicycle parking spaces must be located indoors and within a secure area, such as within an indoor storage area for residents. Storage areas may be provided in garages, carports, storage rooms, and other resident-accessible, secure areas. Spaces within dwelling units or on balconies do not count toward satisfying long-term bicycle parking requirements unless balconies are designed with an enclosed storage area that can accommodate a bicycle, including enclosed balcony storage areas designed with bike hooks.
5. Alternative short-term and long-term bicycle parking designs are acceptable when reviewed and approved by the Director of Development Services.

F. Location of bicycle spaces must meet the following standards:

1. The bicycle parking area must be convenient to building entrances and street access, but must not interfere with normal pedestrian and vehicle traffic.
2. Bicyclists must not be required to travel over stairs to access short-term bicycle parking.
3. Short-term bicycle parking spaces must be located no more than 50 feet from the principal building entrance and at the same grade as the sidewalk or an accessible route.
4. Where short-term bicycle parking spaces have been installed in the right-of-way for public use, such spaces may be counted toward required spaces for all uses within 250 feet of the bike rack.
5. Short-term bicycle parking spaces may be shared between uses. Bicycle parking spaces must be accessible and clearly visible to from all uses. A 25% reduction in the total number of spaces is allowed (*e.g., if two users are required to provide six spaces each, then only nine spaces are required – a 25% reduction of the total of 12 spaces required*).
6. If required short-term bicycle parking facilities are not visible from the street or principal building entrance, signs must be posted indicating their location.

7.2.8 Required Off-Street Loading Facilities

Service drives and other areas must be provided for off-street loading in accordance with this subsection and Table 7.2.6: Off-Street Loading Requirements.

A. Design and Layout

1. The location, design, and layout of all loading spaces must be indicated on required site plans.
2. A loading space must be a minimum of 12 feet by 35 feet and have a minimum vertical clearance of 14 feet.
 - a. Loading spaces for a funeral home may be reduced in size to 10 feet by 25 feet and may have a vertical clearance reduced to eight feet.
3. Any vehicle sales or similar use requiring delivery of vehicles by truck must demonstrate adequate on-site area exists for the loading and unloading of such trucks. Such loading and unloading activity is not permitted in any public right-of-way.
4. Any convenience store or similar use requiring frequent deliveries by truck must demonstrate that an adequate on-site area exists for the loading and unloading of such trucks. Such loading and unloading activity is not permitted in any public right-of-way.
5. Vehicles must have access to loading areas only from arterial or collector roadways, and not from local streets.
6. All off-street loading spaces must be improved with a hard surfaced, all-weather dustless material.
7. No part of a loading area utilized for the access, maneuvering and temporary parking of delivery vehicles may be used for the parking of employee or customer vehicles.
8. No part of a loading area may be utilized for the outdoor storage of materials, merchandise, and equipment.
9. In the process of loading or unloading, no vehicle may block the passage of other vehicles on the service drive or extend into any other public or private drive or street used for traffic circulation.

| Table 7.2.6: Off-Street Loading Requirements | |
|--|-----------------------------------|
| Use Type | Minimum Number of Spaces Required |
| Multi-Family Dwelling | |
| Total of 50 dwelling units or more | 1 loading space |
| Commercial & Institutional Use | |
| 10,000 - 100,000sf GFA | 1 loading space |
| 100,001 - 200,000sf GFA | 2 loading spaces |
| 200,001sf and above GFA | 3 loading spaces |
| Industrial Use | |
| 10,000 - 40,000sf GFA | 1 loading spaces |
| 40,001 - 100,000sf GFA | 2 loading spaces |
| 100,001 and above GFA | 3 loading spaces |

B. Loading Adjacent to Residential Districts

When a loading dock abuts a single-family residential zoning district:

1. Hours of operation, including loading and unloading of supplies or merchandise for commercial uses, are limited between the hours of 7:00 a.m. and 10:00 p.m.
2. Loading docks must be signed to indicate “no idling.”

C. Landscaping and Screening

1. All loading areas are required to provide a parking buffer in accordance with Section 7.3.
2. The following standards apply to all sites with loading docks in non-industrial districts. Loading docks must be:
 - a. Located at the side or rear of buildings a minimum of 50 feet away from any single-family dwelling use, unless the loading area is wholly within a closed building.
 - b. Screened from view from residential properties or public rights-of-way.

7.2.9 Commercial Vehicle Storage

A. Residential Lots

1. No commercial vehicle may be parked outdoors on a residential lot, with the exception of vehicles engaged in loading or unloading or current work being done to the adjacent premises.
 - a. However, this does not include standard size passenger motor vehicles (including, but not limited to, vans, sports utility vehicles (SUVs), standard passenger size livery vehicles, and pick-up trucks), which may be stored or parked outdoors overnight on lots in residential districts.
 - b. Permitted standard size passenger commercial vehicles, as described in item i above, also include those owned and used for commercial purposes by the occupant of a dwelling or guest, provided that the vehicle is stored or parked in a permitted parking area. Permitted personal commercial vehicles may include the logo of the commercial business painted on or applied to the vehicle. Any commercial vehicle that exceeds 1.5 tons in capacity is not considered a standard size passenger commercial vehicle.
2. All other commercial vehicles that exceed standard size passenger vehicles including, but not limited to, semi-truck tractor units, with or without attached trailers, commercial trailers, flatbed trucks, box vans and box trucks, sprinter vans, buses, tow trucks, construction vehicles, livery vehicles that exceed standard passenger vehicle size, such as limousines, or other large commercial vehicles are not permitted to be parked outdoors overnight on a residential lot.

B. Nonresidential Lots

On nonresidential lots, commercial vehicles with the logo of the commercial business painted on or applied to the vehicle that are being operated and stored in the normal course of business, such as signs located on delivery trucks, promotional vehicles, moving vans, and rental trucks, are permitted to be stored on the lot in areas related to their use as vehicles, provided that the primary purpose of such vehicles is not the display of signs. All such vehicles must be in operable condition.